

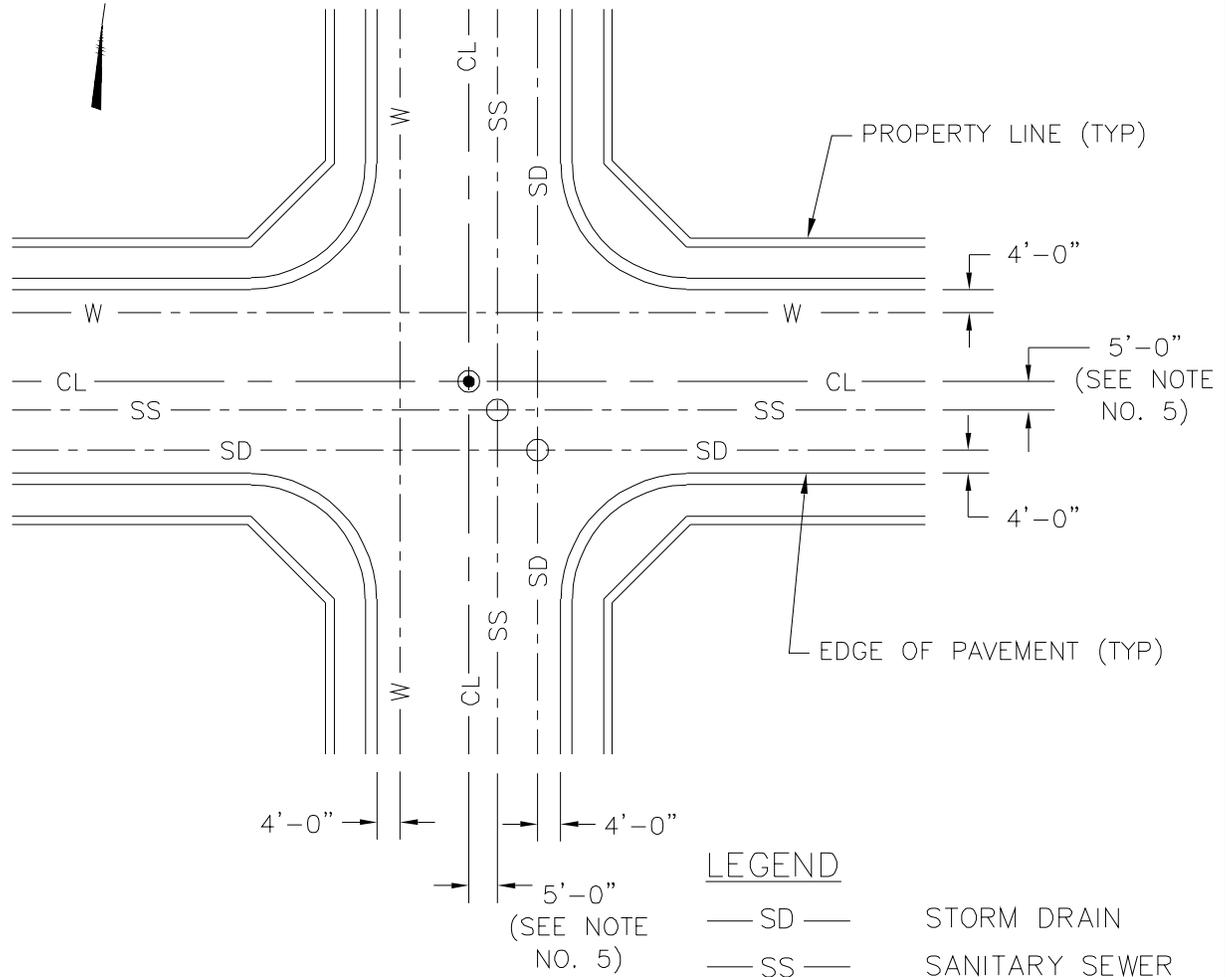
**NOTES:**

- THE MAXIMUM LENGTH OF CUL-DE-SACS SHALL BE 500'-0" FROM FACE OF CURB TO THE CENTER OF BULB. ONLY IN INDUSTRIALLY ZONED AREAS MAY THE LENGTH BE INCREASED TO A MAXIMUM OF 1000'-0", PROVIDED THE FOLLOWING CRITERIA ARE MET:
  - STREET RIGHT-OF-WAY OF 60'-0" WITH 40'-0" BETWEEN CURBS.
  - TURN-AROUND CURB RADIUS OF 53'-0".
  - EMERGENCY ACCESS TO ANOTHER PUBLIC STREET MAY BE REQUIRED BY THE FIRE MARSHAL.
- GUTTER SLOPE AROUND BULBS SHALL BE 0.35% MINIMUM.
- BULBS MAY BE OFFSET TO EITHER SIDE.
- A 10'-0" EASEMENT IS REQUIRED FOR PUBLIC UTILITIES AND STREET TREE PLANTING.
- WATER MAINS MAY BE REQUIRED TO BE "LOOPE" BY THE CITY ENGINEER. SEE WATER SERVICE DRAWINGS FOR APPLICATIONS REQUIRED ON CUL-DE-SACS.

REV. NO.	REV. DATE	REV. BY	<b>MINIMUM STANDARD CUL-DE-SAC</b>	REVISION APPROVED BY CITY ENGINEER	
				Finbar J. O'Regan DATE: 11/25/03	
DIGITIZED	6/1/2003		<b>CITY OF STOCKTON</b> DEPARTMENT OF PUBLIC WORKS	SUPERCEDES DWG. DATED	DRAWING NO.
DWG. BY		SCALE		01/09/02	20
CK. BY		NONE			



ALL DIMENSIONS ARE MEASURED TO THE CENTERLINE OF PIPE AS SHOWN.



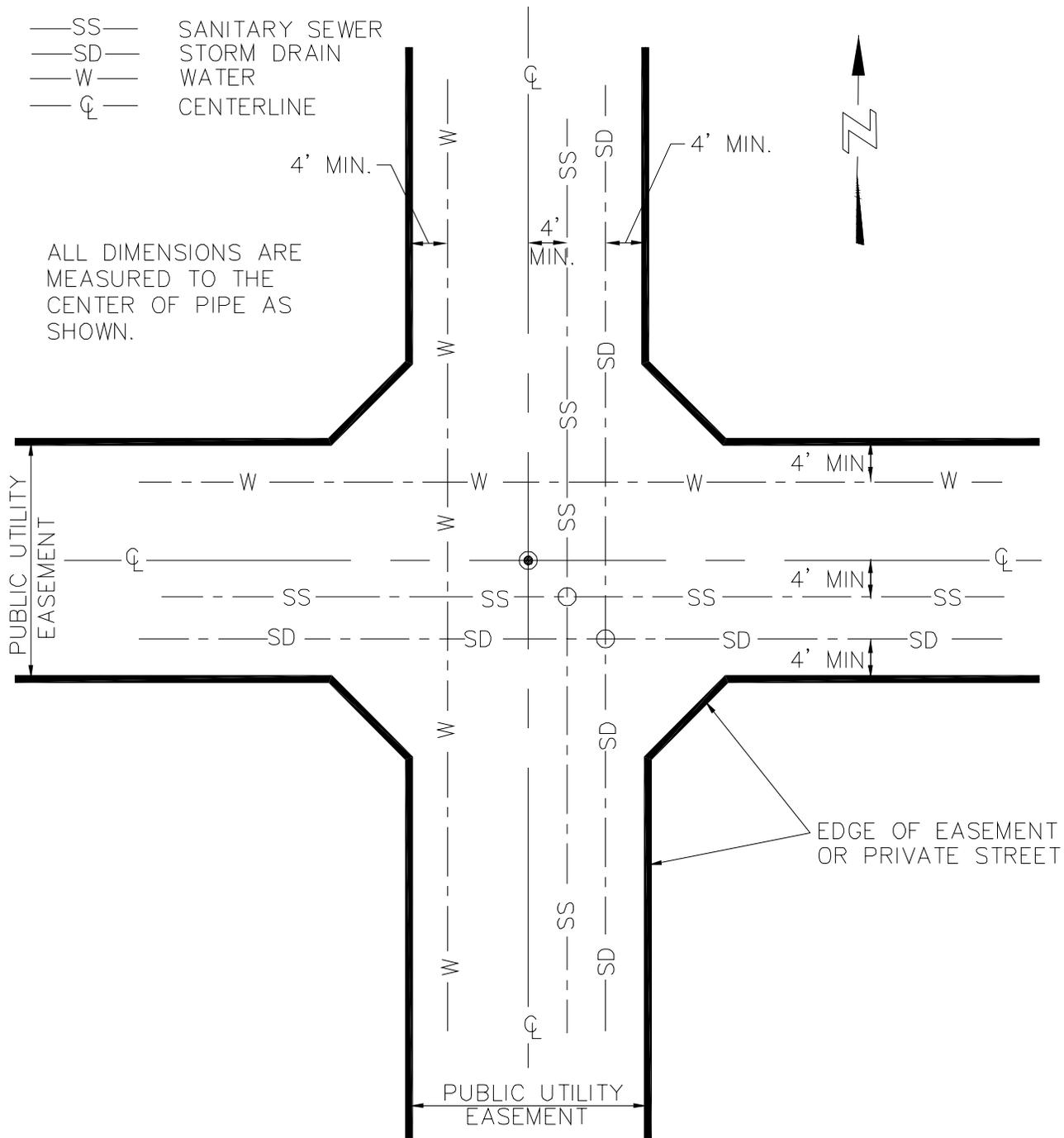
- LEGEND**
- SD — STORM DRAIN
  - SS — SANITARY SEWER
  - W — WATER LINE
  - CL — CENTERLINE

**NOTES:**

1. SEE DRAWING NO. 5 FOR LOCATION OF ELECTRIC, TELEPHONE, GAS, TV CONDUIT AND STREET LIGHT CONDUIT.
2. SANITARY SEWER AND STORM DRAIN LINES SHALL BE LOWER THAN WATER MAINS. FOR PARALLEL AND PERPENDICULAR CONSTRUCTION, SEE DWG. NO.'S 47, 48 & 49.
3. IN COMMERCIAL OR INDUSTRIAL SUBDIVISIONS WHERE WATER SERVICE SIZES AND LOCATIONS ARE UNKNOWN AT THE TIME THE WATER MAIN IS INSTALLED, THE WATER MAIN MAY BE LOCATED IN THE P.U.E. 2 FEET FROM THE PROPERTY LINE WITH THE APPROVAL OF THE CITY ENGINEER. NO STREET CUTS SHALL BE PERMITTED FOR WATER SERVICES TO THE OPPOSITE SIDE OF THE STREET.
4. 10-FOOT MINIMUM SEPARATION BETWEEN WATER LINE AND ANY SANITARY SEWER OR STORM DRAIN LINES AS MEASURED FROM THE OUTSIDE OF THE PIPES.
5. RELOCATE SEWER TO  $\odot$  AS REQUIRED TO MAINTAIN REQUIRED SEPARATION ON LOCAL STREET SECTIONS.

REV. NO.	REV. DATE	REV. BY	<p style="color: red; font-size: 1.2em;">STREET LOCATION OF UNDERGROUND UTILITIES</p> <p style="color: blue; font-size: 1.2em;">CITY OF STOCKTON</p> <p style="color: blue;">DEPARTMENT OF PUBLIC WORKS</p>	<p style="color: blue; font-size: 0.8em;">REVISION APPROVED BY CITY ENGINEER</p> <p style="color: blue; font-size: 1.2em; font-family: cursive;">Finbar J. O'Regan</p> <p style="color: blue; font-size: 0.8em;">DATE: 11/25/03</p>	
4	6/1/2003	HL/EA		<p style="color: blue; font-size: 0.8em;">SUPERCEDES DWG. DATED</p> <p style="color: blue; font-size: 0.8em;">01/09/02</p>	<p style="color: blue; font-size: 0.8em;">DRAWING NO.</p> <p style="color: red; font-size: 1.5em; font-weight: bold;">21</p>
<p style="color: blue; font-size: 0.8em;">DIGITIZED</p> <p style="color: blue; font-size: 0.8em;">1/1/92</p>					
DWG. BY	WW	SCALE			
CK. BY	RH	NONE			

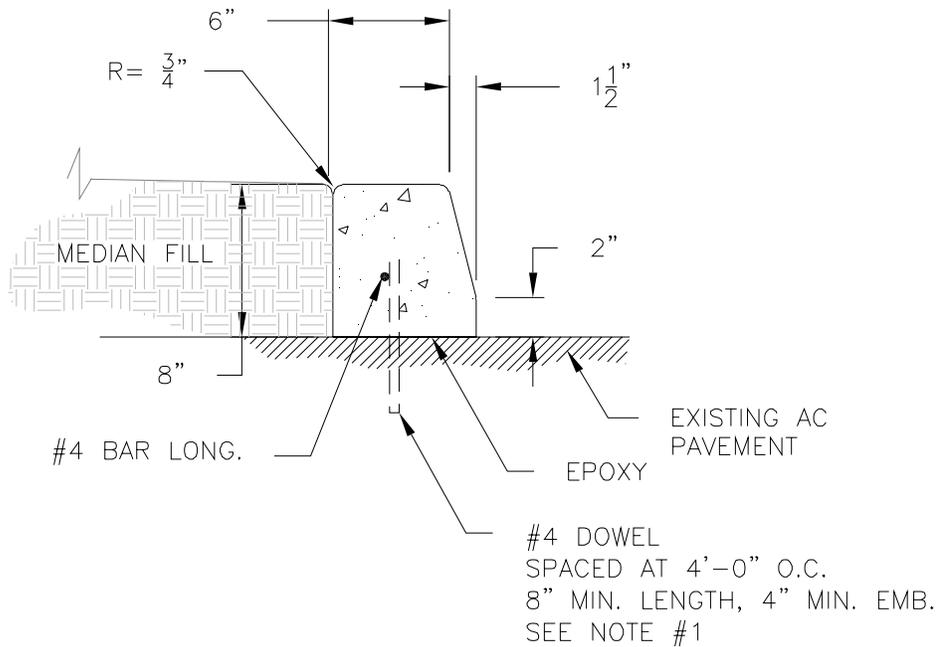
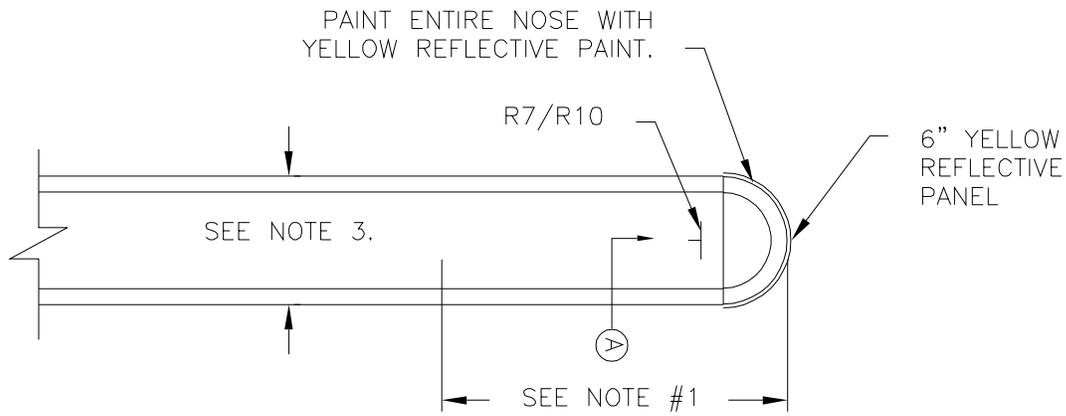
— SS — SANITARY SEWER  
 — SD — STORM DRAIN  
 — W — WATER  
 — C — CENTERLINE



**NOTE**

1. ALL PRIVATE UTILITIES TO BE LOCATED OUTSIDE OF LIMITS SHOWN UNLESS APPROVED BY THE CITY ENGINEER.
2. 10-FOOT MINIMUM SEPARATION BETWEEN WATER LINE AND ANY SEWER LINE AS MEASURED FROM THE OUTSIDE OF THE PIPES.

REV. NO.	REV. DATE	REV. BY	PUBLIC UTILITY LOCATIONS IN PRIVATE STREETS OR EASEMENTS	REVISION APPROVED BY CITY ENGINEER	
3	6/1/2000	HLE/RH		Finbar J. O'Regan	
DIGITIZED	7/1/92			DATE: 01/09/02	
DWG. BY	WW	SCALE	CITY OF STOCKTON DEPARTMENT OF PUBLIC WORKS	SUPERCEDES DWG. DATED	DRAWING NO.
CK. BY	RH	NONE		6/1/94	21A

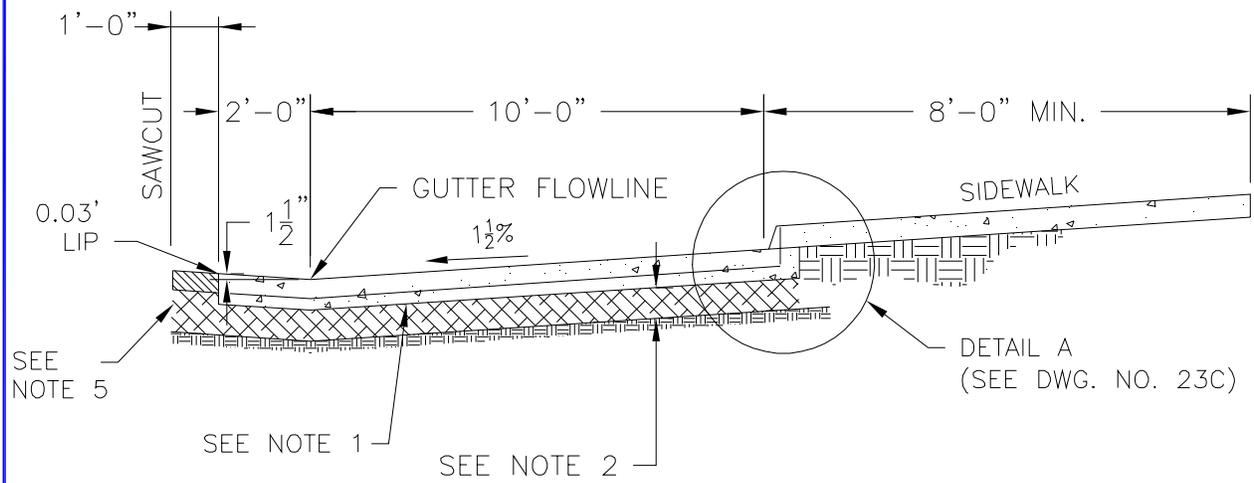
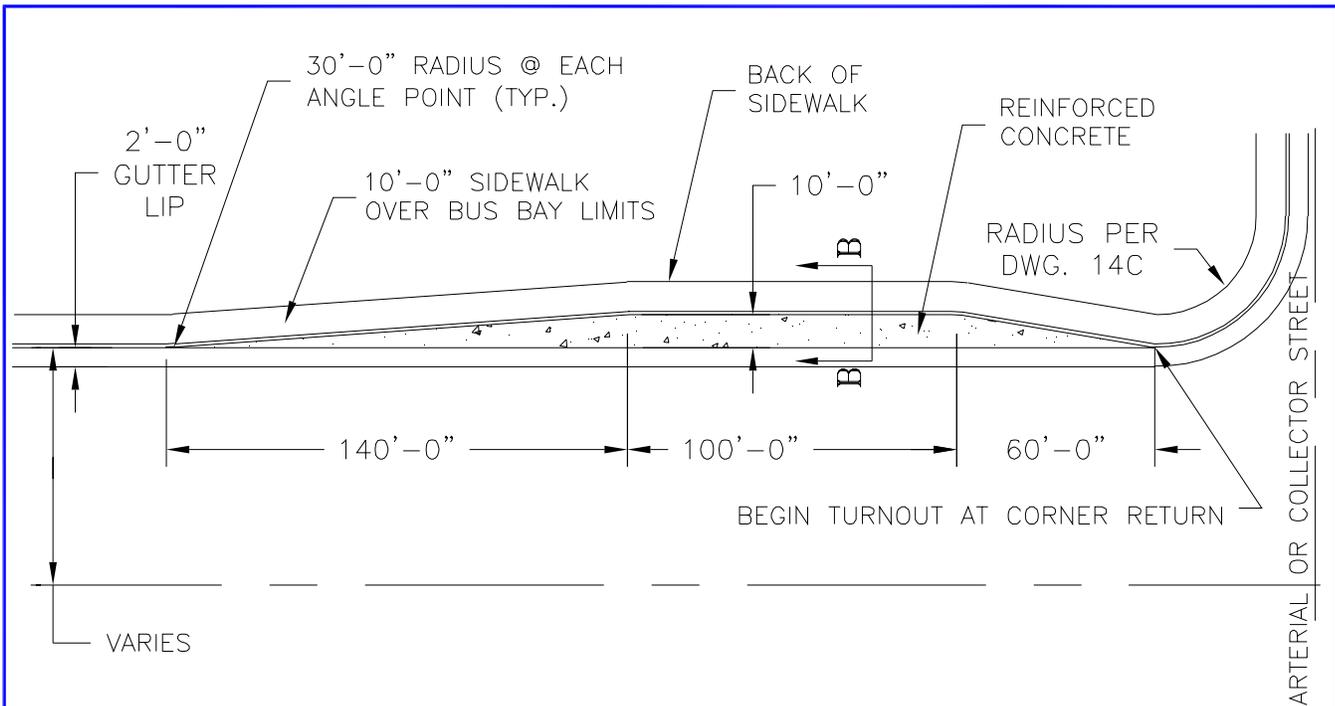


(A) MEDIAN CURB - TYPE 4  
N.T.S

NOTES:

1. PLACE STEEL DOWELS WITHIN THE FIRST 20'-0" OF MEDIAN FROM ANY END.
2. FOR APPLICATION INSTRUCTIONS, FOLLOW CALTRANS STANDARD SECTION 73-1.05B.
3. MINIMUM MEDIAN 2'-0" WIDTH, UNLESS APPROVED BY THE CITY ENGINEER.

REV. NO.	REV. DATE	REV. BY	MEDIAN CURBING OVER EXISTING PAVEMENT	REVISION APPROVED BY CITY ENGINEER <i>Finbar J. O'Regan</i> DATE: 11/25/03	
DIGITIZED	6/1/2003			CITY OF STOCKTON DEPARTMENT OF PUBLIC WORKS	
DWG. BY	CG	SCALE	CITY OF STOCKTON DEPARTMENT OF PUBLIC WORKS	SUPERCEDES DWG. DATED	DRAWING NO.
CK. BY		NONE			22



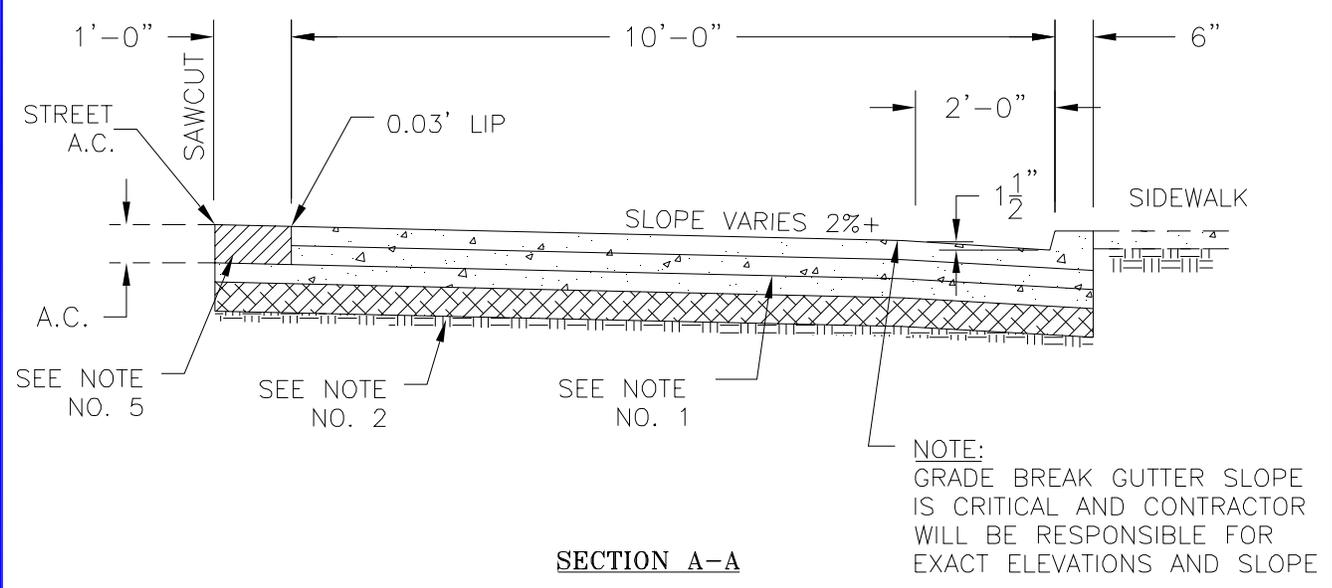
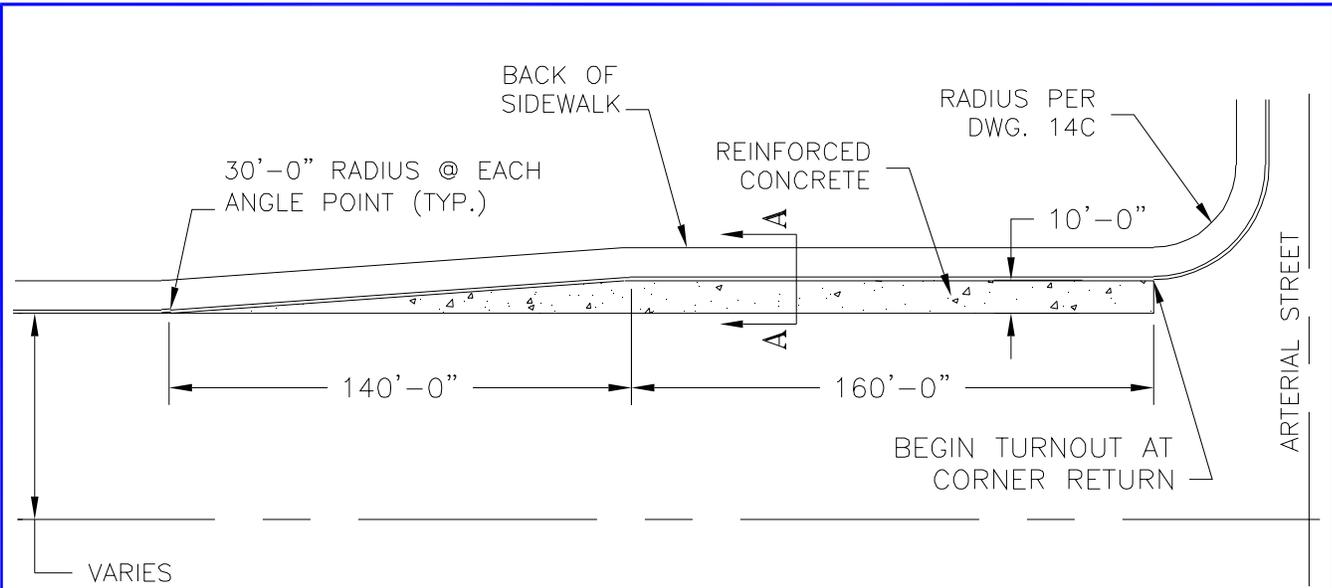
**SECTION B-B \***

- \* BUS TURNOUT CROSS SECTION SHOWN FOR COLLECTOR STREETS CAN ALSO BE USED FOR ARTERIAL STREETS.
- \* SEE DWG. 23C FOR NOTES.

CRITERIA FOR INSTALLATION:

BUS TURNOUTS SHALL BE INSTALLED ON ALL FOUR DEPARTURE LEGS WHEN AN ARTERIAL STREET INTERSECTS WITH AN ARTERIAL STREET OR COLLECTOR STREET. BUS TURNOUTS AT THE INTERSECTION OF AN ARTERIAL STREET AND LOCAL STREET SHALL BE INSTALLED ON A CASE-BY-CASE BASIS WITH SJRTD RECOMMENDATION.

REV. NO.	REV. DATE	REV. BY	<b>BUS TURNOUTS ARTERIAL STREETS</b>	REVISION APPROVED BY CITY ENGINEER <i>Finbar J. O'Regan</i> DATE: 11/25/03	
DIGITIZED	6/1/2003			SUPERCEDES DWG. DATED	
DWG. BY	HLE	SCALE	<b>CITY OF STOCKTON</b> DEPARTMENT OF PUBLIC WORKS	DRAWING NO. <span style="font-size: 2em; color: red;">23A</span>	
CK. BY	TG	NONE			



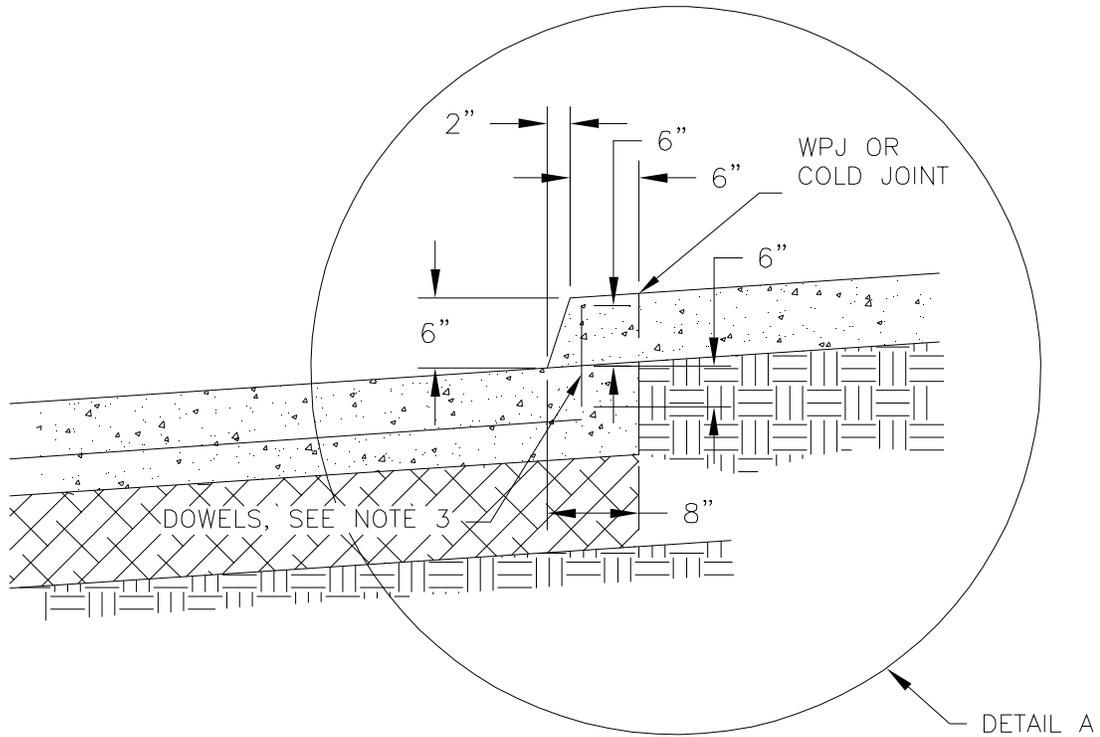
**SECTION A-A**

SEE DWG. 23C FOR NOTES.

CRITERIA FOR INSTALLATION:

- BUS TURNOUTS SHALL BE INSTALLED ON ALL FOUR DEPARTURE LEGS WHEN A COLLECTOR STREET INTERSECTS WITH AN ARTERIAL STREET. BUS TURNOUTS AT THE INTERSECTION OF TWO COLLECTOR STREETS SHALL BE INSTALLED ON A CASE-BY-CASE BASIS WITH

REV. NO.	REV. DATE	REV. BY	SJRTD RECOMMENDATION. <b>BUS TURNOUTS</b> <b>COLLECTOR STREETS</b>	REVISION APPROVED BY CITY ENGINEER <i>Finbar J. O'Regan</i> DATE: 11/25/03	
DIGITIZED	6/1/2003			SUPERCEDES DWG. DATED	
DWG. BY	HLE	SCALE	<b>CITY OF STOCKTON</b> DEPARTMENT OF PUBLIC WORKS	DRAWING NO. <b>23B</b>	
CK. BY	TG	NONE			

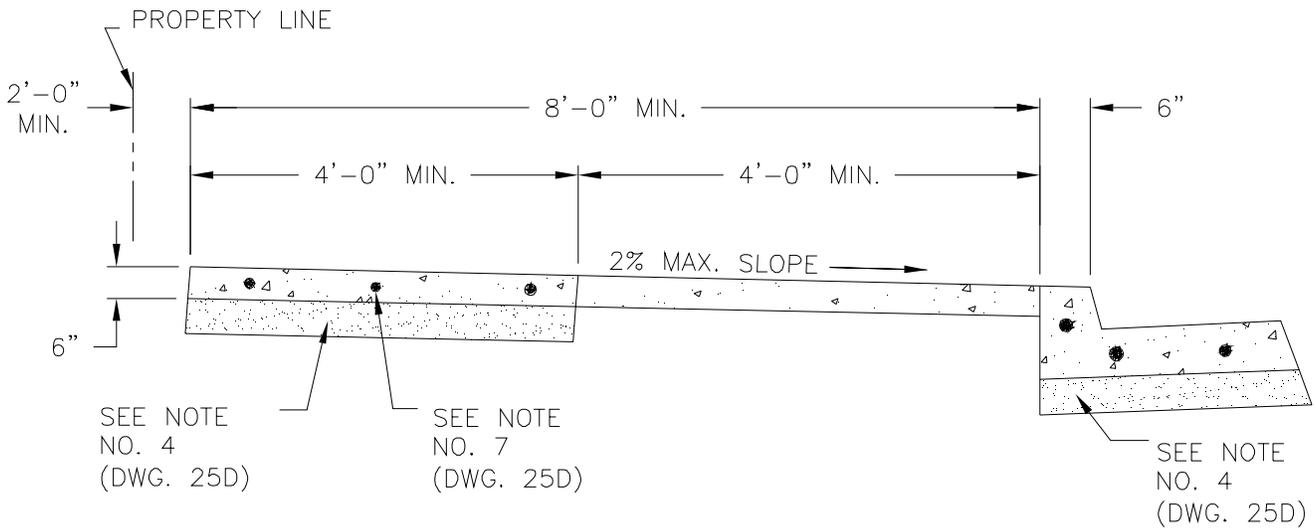


**DETAIL A**

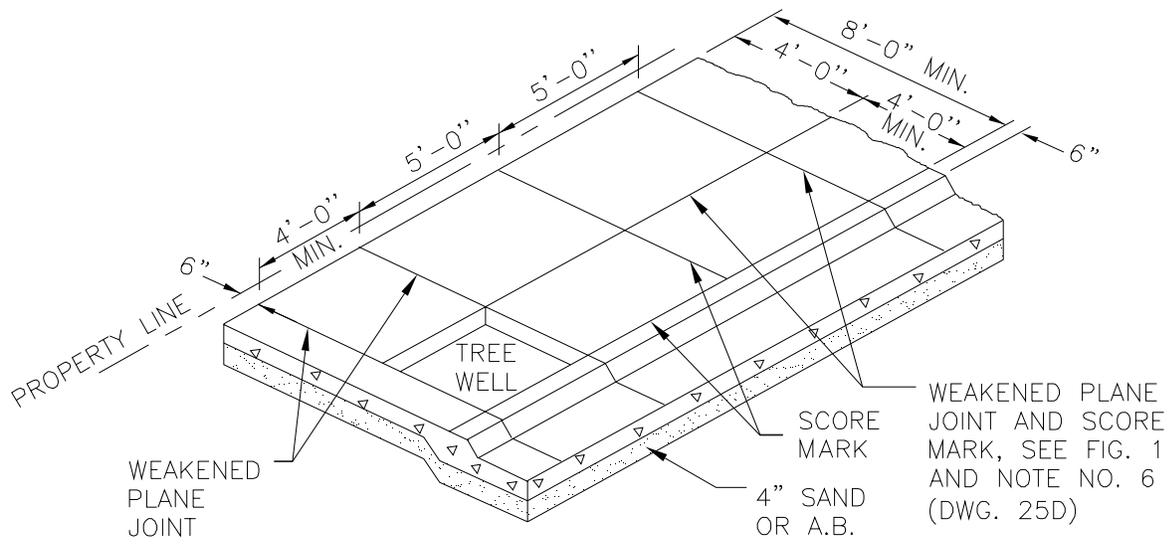
**NOTES:**

- 1) CONCRETE SHALL BE NINE INCHES THICK WITH #4, GRADE 60 REBAR AT 2'-0" CENTER EACH WAY. USE THREE INCH DOBIES (REBAR SPACERS WITH WIRE TIES). CONCRETE SHALL BE CLASS A, SIX SACK, ALL FINISH SHALL BE MEDIUM BROOM.
- 2) PLACE SIX INCHES OF 3/4" AGGREGATE BASE AT 95% RELATIVE COMPACTION.
- 3) THE SLAB FOR THE BUS TURNOUT MAY BE PLACED MONOLITHICALLY WITH THE CURB AND SIDEWALK (PREFERRED). IF CONCRETE IS PLACED WITH A COLD JOINT BETWEEN THE BOTTOM OF THE CURB AND THE TURNOUT SLAB, THEN THE CURB SHALL BE DOWELED (WET SET) TO THE SLAB WITH #4, GRADE 60 REBAR AT FOUR FOOT CENTERS. CURB SHALL ALSO BE EPOXYED TO THE SLAB
- 4) PLACE 2 INCH DEEP TOOL JOINTS AT 10 FOOT CENTERS, PERPENDICULAR TO THE LIP OF GUTTER IN THE TURNOUT SLAB AND EXTENDED PERPENDICULAR TO THE FACE OF CURB IN THE CURB AND SIDEWALK.
- 5) EXISTING STRUCTURAL SECTION OR PER PLANS WHICHEVER IS GREATER.

REV. NO.	REV. DATE	REV. BY	<b>BUS TURNOUTS</b>	REVISION APPROVED BY CITY ENGINEER <i>Finbar J. O'Regan</i> DATE: 11/25/03	
DIGITIZED	6/1/2003			CITY OF STOCKTON DEPARTMENT OF PUBLIC WORKS	SUPERCEDES DWG. DATED
DWG. BY	HLE	SCALE			
CK. BY	TG	NONE			



SECTION AT TREEWELL



VERTICAL CURB, GUTTER & SIDEWALK WITH TREE WELLS

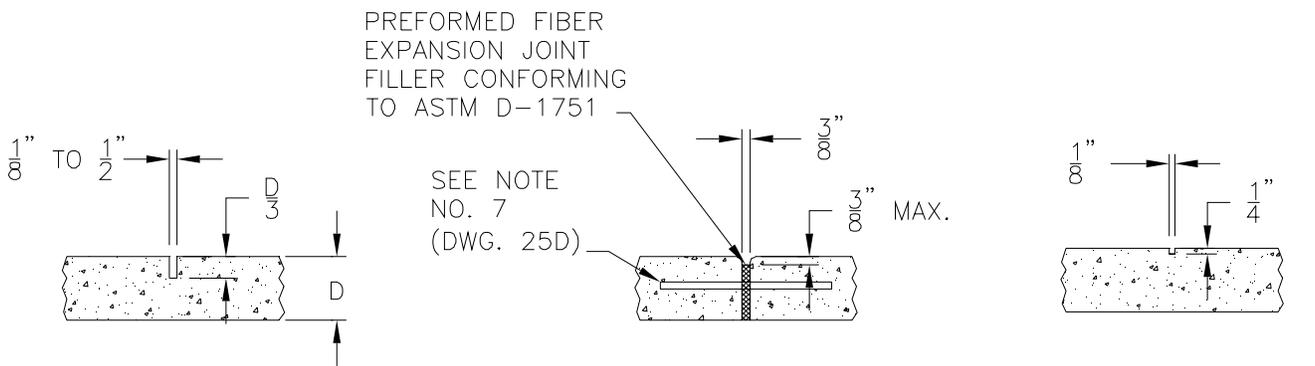


FIG. 1 WEAKENED PLANE JOINT

FIG. 2 EXPANSION JOINT

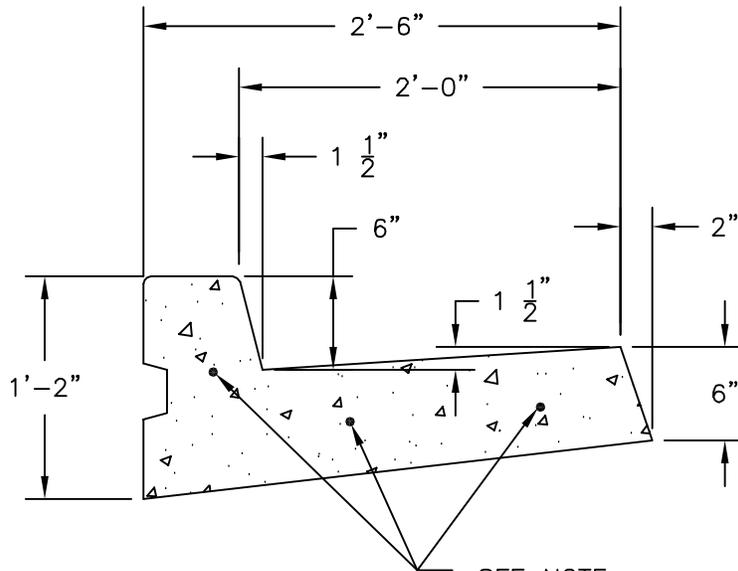
FIG. 3 SCORE LINE

REV. NO.	REV. DATE	REV. BY
8	6/1/2003	HL/EA
DIGITIZED 7/1/90		
DWG. BY	RC	SCALE
CK. BY		NONE

**SIDEWALK DETAILS**  
**CITY OF STOCKTON**  
 DEPARTMENT OF PUBLIC WORKS

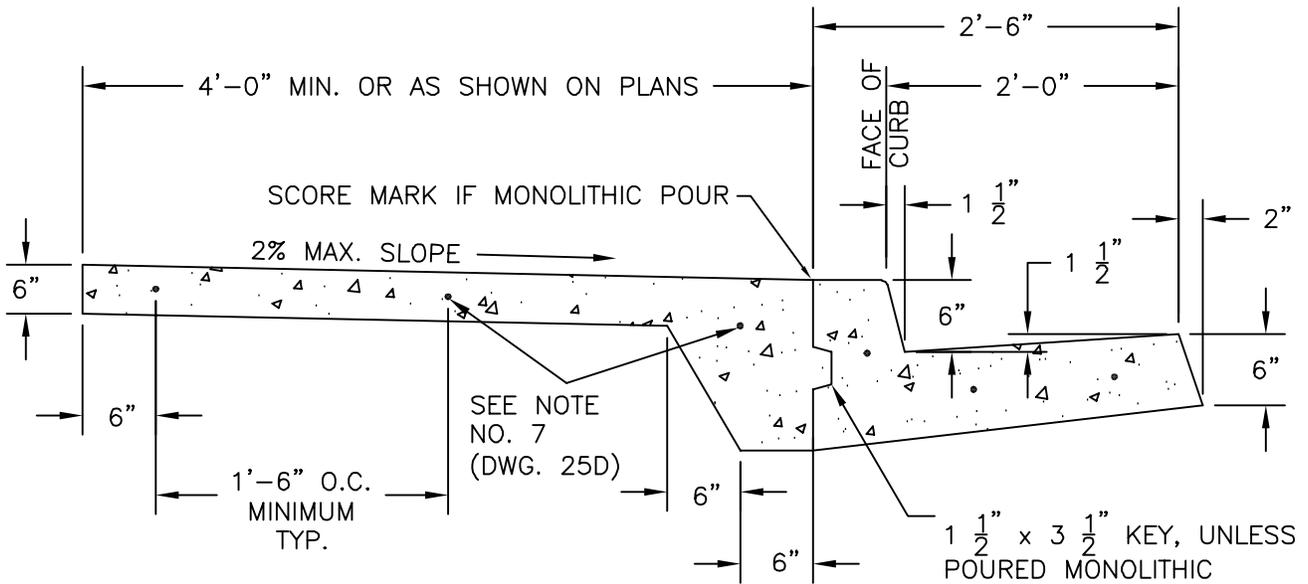
REVISION APPROVED BY CITY ENGINEER	
<i>Finbar J. O'Regan</i>	
DATE: 11/25/03	
SUPERCEDES DWG. DATED	DRAWING NO.
01/09/02	25





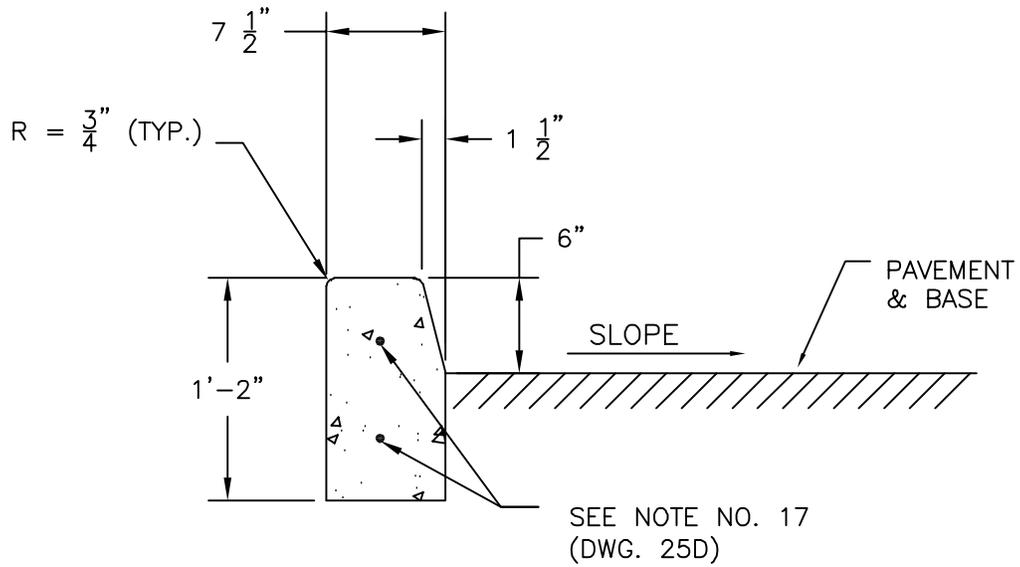
SEE NOTE  
NO. 16  
(DWG. 25D)

VERTICAL CURB & GUTTER

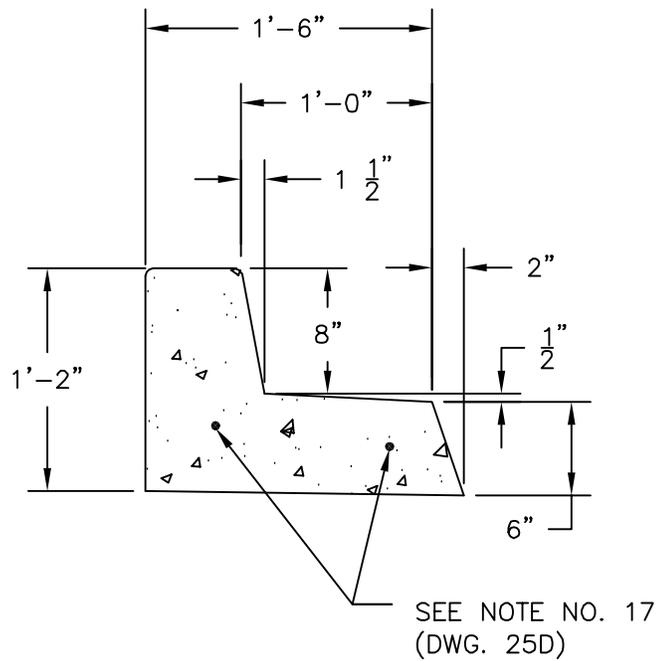


VERTICAL CURB, GUTTER & SIDEWALK

REV. NO.	REV. DATE	REV. BY	VERTICAL CURB, GUTTER & SIDEWALK	REVISION APPROVED BY CITY ENGINEER	
1	6/1/2003	HL/EA		Finbar J. O'Regan	
DIGITIZED			CITY OF STOCKTON DEPARTMENT OF PUBLIC WORKS	DATE:	11/25/03
DWG. BY	SCALE	NONE		SUPERCEDES DWG. DATED	DRAWING NO.
CK. BY					



TYPE "B" VERTICAL CURB



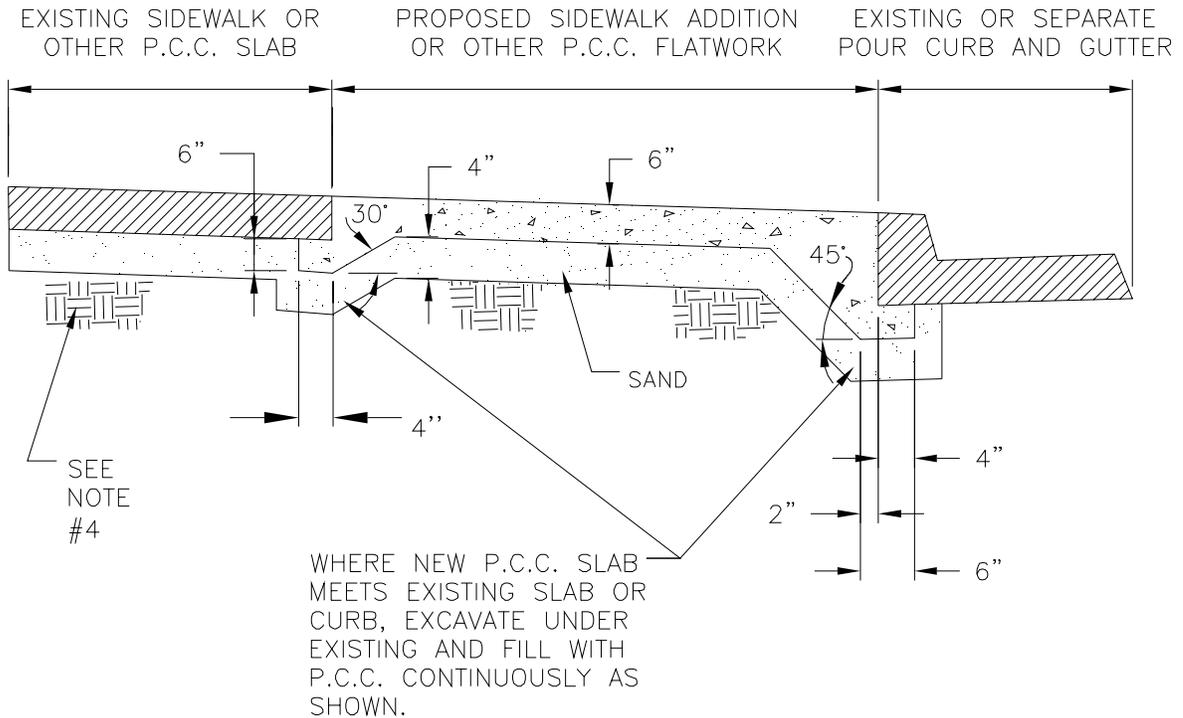
TYPE "F" CURB & APRON

REV. NO.	REV. DATE	REV. BY	VERTICAL CURB CURB & APRON	REVISION APPROVED BY CITY ENGINEER
1	6/1/2003	HL/EA		Finbar J. O'Regan DATE: 11/25/03
DIGITIZED			CITY OF STOCKTON DEPARTMENT OF PUBLIC WORKS	SUPERCEDES DWG. DATED
DWG. BY	SCALE	NONE		DRAWING NO. 25C
CK. BY				

NOTES:

1. CURB, GUTTER, AND ALL P.C.C. FLATWORK TO HAVE A LIGHT BROOM FINISH.
2. CONSTRUCT EXPANSION JOINTS 150'-0" ON CENTER MAXIMUM, AND AT RETURNS, LIGHT POLES, HYDRANTS, CATCH BASINS, BOTH SIDES OF DRIVEWAY, AND OTHER FIXED OBJECTS.
3. CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE APPLICABLE SECTIONS OF THE CURRENT CITY OF STOCKTON STANDARD SPECIFICATIONS.
4. 4" SAND OR AGGREGATE BASE. ALL MATERIAL UNDER SAND BASE SHALL BE SCARIFIED FOR A 6" MINIMUM DEPTH AND COMPACTED TO 90% RELATIVE COMPACTION.
5. SEE DEFINITION SECTION OF STANDARD SPECIFICATIONS FOR DEFINITION OF SAND.
6. WEAKENED PLANE JOINTS AND SCORE MARKS AS SHOWN. SEE FIGURE 1 FOR WEAKENED PLANE JOINT WIDTH AND DEPTH.
7. PLACE 5/8" x 24" LONG STEEL DOWELS THROUGH EVERY EXPANSION JOINT SPACED AT 1'-6" ON CENTER (MIN.), GREASED AND WRAPPED ON ONE SIDE, OFFSET 6" FROM CONCRETE EDGES, UNLESS OTHERWISE SHOWN OR SPECIFIED. MINIMUM THREE DOWELS IN 4' WIDE SIDEWALK.
8. CURB, GUTTER AND SIDEWALK CONSTRUCTION SHALL CONFORM TO SECTION 73, STANDARD SPECIFICATIONS, CALIFORNIA DEPARTMENT OF TRANSPORTATION (CALTRANS), EXCEPT AS MODIFIED HEREIN.
9. SUBGRADE FOR CURB, GUTTER, SIDEWALK AND DRIVEWAYS SHALL BE COMPACTED TO A MINIMUM RELATIVE COMPACTION OF 90% TO A DEPTH OF 6". WHERE THE SUBGRADE "R" VALUE IS LESS THAN 50, PLACE 4" MINIMUM OF AGGREGATE SUBBASE CLASS IV UNDER THE CONCRETE SECTIONS AND COMPACT TO A MINIMUM OF 90%.
10. ALL RADII FOR ROUNDING EDGES SHALL BE 3/4" UNLESS NOTED.
11. CONCRETE SHALL BE CLASS B, PER CALTRANS SECTION 90.
12. EXPANSION JOINTS AND WEAKENED PLANE JOINTS SHALL BE INSTALLED AS INDICATED ON THE PLANS OR STANDARD DETAILS.
13. CURB, GUTTER AND SIDEWALK SHALL HAVE A FINE HAIR BROOM FINISH; CURB AND GUTTER PARALLEL TO THE FLOW LINE.
14. DEPRESS A 2" HIGH LETTER "W" OR "S" 1/4" DEEP INTO THE TOP OF THE CURB TO IDENTIFY SERVICE LOCATIONS.
15. DURING CONSTRUCTION OF GUTTERS, WATER SHALL BE USED TO INSURE PROPER DRAINAGE ALONG THE FLOWLINE.
16. 3-5/8" x 24" LONG STEEL DOWELS MINIMUM THROUGH EVERY EXPANSION JOINT.
17. 2-5/8" x 24" LONG STEEL DOWELS MINIMUM THROUGH EVERY EXPANSION JOINT.

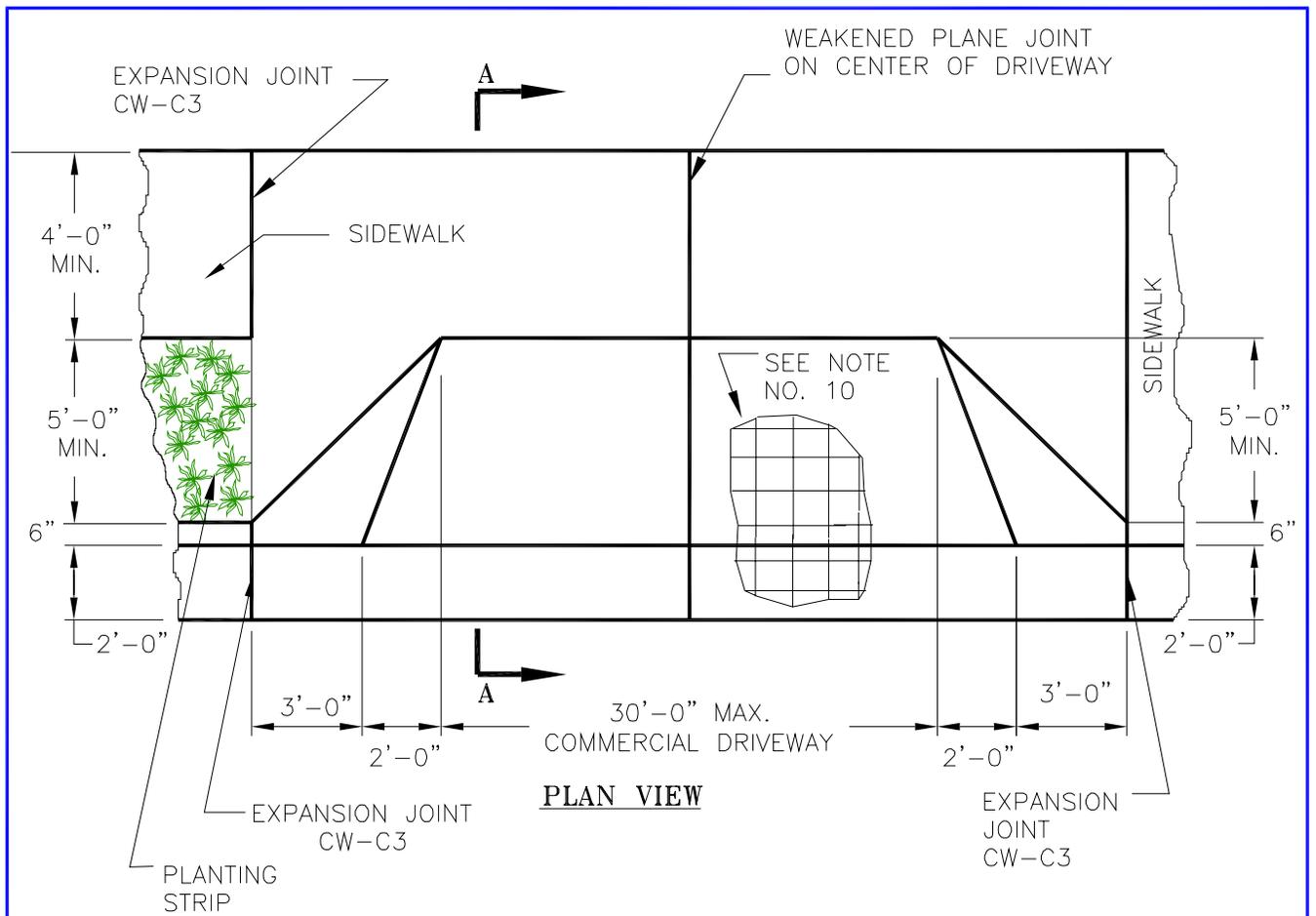
REV. NO.	REV. DATE	REV. BY	<b>CONCRETE CURB, GUTTER AND SIDEWALK CONSTRUCTION STANDARDS</b>	REVISION APPROVED BY CITY ENGINEER <i>Finbar J. O'Regan</i> DATE: 11/25/03	
DIGITIZED	6/1/2003				
DWG. BY	HL	SCALE	<b>CITY OF STOCKTON DEPARTMENT OF PUBLIC WORKS</b>	SUPERCEDES DWG. DATED	DRAWING NO.
CK. BY		NONE			<b>25D</b>



NOTES:

1. IF THE NEW P.C.C. SLAB IS POURED MONOLITHICALLY WITH THE CURB AND GUTTER, OR IF THE CURB, GUTTER, AND SLAB ARE KEYED IN ACCORDANCE WITH THE COUNTYWIDE STANDARD (CW-C7), THE ABOVE KEY WILL NOT BE REQUIRED.
2. THE ABOVE KEY SHALL BE REQUIRED BETWEEN EXISTING AND PROPOSED P.C.C. AT ALL DRIVEWAYS.
3. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE APPLICABLE SECTIONS OF THE CURRENT CITY OF STOCKTON STANDARD SPECIFICATIONS.
4. MATERIAL UNDER ALL SAND BASE SHALL BE COMPACTED TO 90% RELATIVE COMPACTION FOR A MINIMUM DEPTH OF 6".
5. CURB, GUTTER, AND ALL P.C.C FLATWORK TO HAVE A FINE HAIR BROOM FINISH.

REV. NO.	REV. DATE	REV. BY	<p style="color: red; font-size: 1.2em;">KEYING CONCRETE SLABS</p> <p style="font-size: 1.2em;">CITY OF STOCKTON</p> <p style="font-size: 1.2em;">DEPARTMENT OF PUBLIC WORKS</p>	<p>REVISION APPROVED BY CITY ENGINEER</p> <p><i>Finbar J. O'Regan</i></p> <p>DATE: 11/25/03</p>	
5	6/1/2003	HL/EA		<p>SUPERCEDES</p> <p>DWG. DATED</p> <p>1/9/02</p>	
DIGITIZED		7/1/91		<p>DRAWING NO.</p> <p style="font-size: 1.5em;">26</p>	
DWG. BY	RC	SCALE			
CK. BY		NONE			

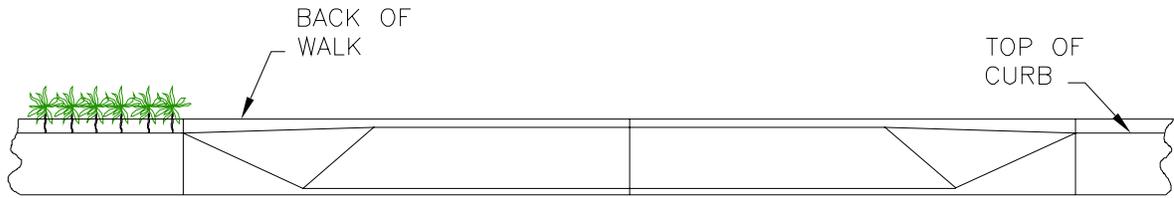


**NOTES:**

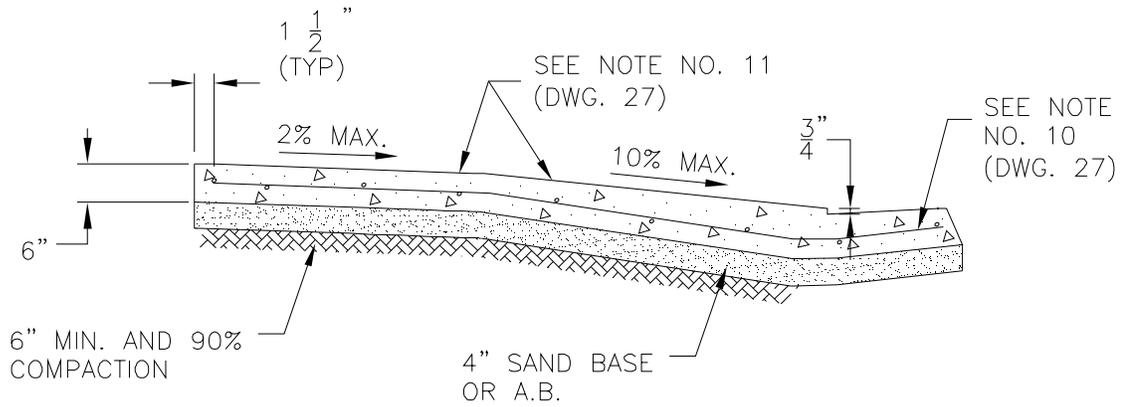
1. THIS DRIVEWAY TO BE USED ONLY WHERE APPROVED BY THE CITY ENGINEER.
2. DRIVEWAY TO CONFORM WITH EXISTING SIDEWALK. ALL CONCRETE TO BE CLASS B (5 SACK).
3. IF SIDEWALK IN BACK EXISTS, WHERE NEW RAMP TO BE PLACED, IT SHALL BE REMOVED AND REPLACED WITH 6" THICK PORTLAND CEMENT CONCRETE.
4. WHERE HIGH STREET CROWN EXISTS, DRIVEWAY RAMP MAY BE EXTENDED FROM BACK OF WALK TO LIP.
5. WHERE DRIVEWAY PROVIDES ACCESS TO A ONE-WAY STREET, APPROPRIATE SIGNS AS SPECIFIED BY THE CITY TRAFFIC ENGINEER WILL BE REQUIRED.
6. DRIVEWAY RAMP SHALL BE AS WIDE AS EXISTING DRIVEWAY INTO YARD, ROUNDED UP TO NEXT EVEN FOOT (10'-0" MINIMUM).
7. IN EVENT OF OBSTRUCTIONS IN PARKWAY (E.G., POWER POLES, ETC.) WIDTH MAY BE MODIFIED TO MEET EXISTING CONDITIONS AS APPROVED BY THE CITY ENGINEER.
8. PARKWAY WARP WINGS SHALL BE 3'-0" FOR 6" CURB OR HIGHER.
9. WHEN IT IS NOT POSSIBLE TO PROVIDE 2'-0" OF FULL HEIGHT WITH STANDARD 3'-0" WARPS ON EITHER SIDE BETWEEN ADJACENT DRIVEWAYS, A COMMON DRIVEWAY SHALL BE INSTALLED.
10. #4 BAR AT 18" ON CENTER, EACH WAY.
11. FINE HAIR BROOM FINISH.

REV. NO.	REV. DATE	REV. BY	<b>COMMERCIAL RAMP DRIVEWAY</b>	REVISION APPROVED BY CITY ENGINEER	
5	6/1/2000	HLE/RH		Finbar J. O'Regan DATE: 01/09/02	
DIGITIZED		7/1/91	<b>CITY OF STOCKTON</b> DEPARTMENT OF PUBLIC WORKS	DRAWING NO.	
DWG. BY	RC	SCALE		27	
CK. BY		NONE		SUPERCEDES DWG. DATED 2/23/95	

(CONT'D FROM DWG. 27)

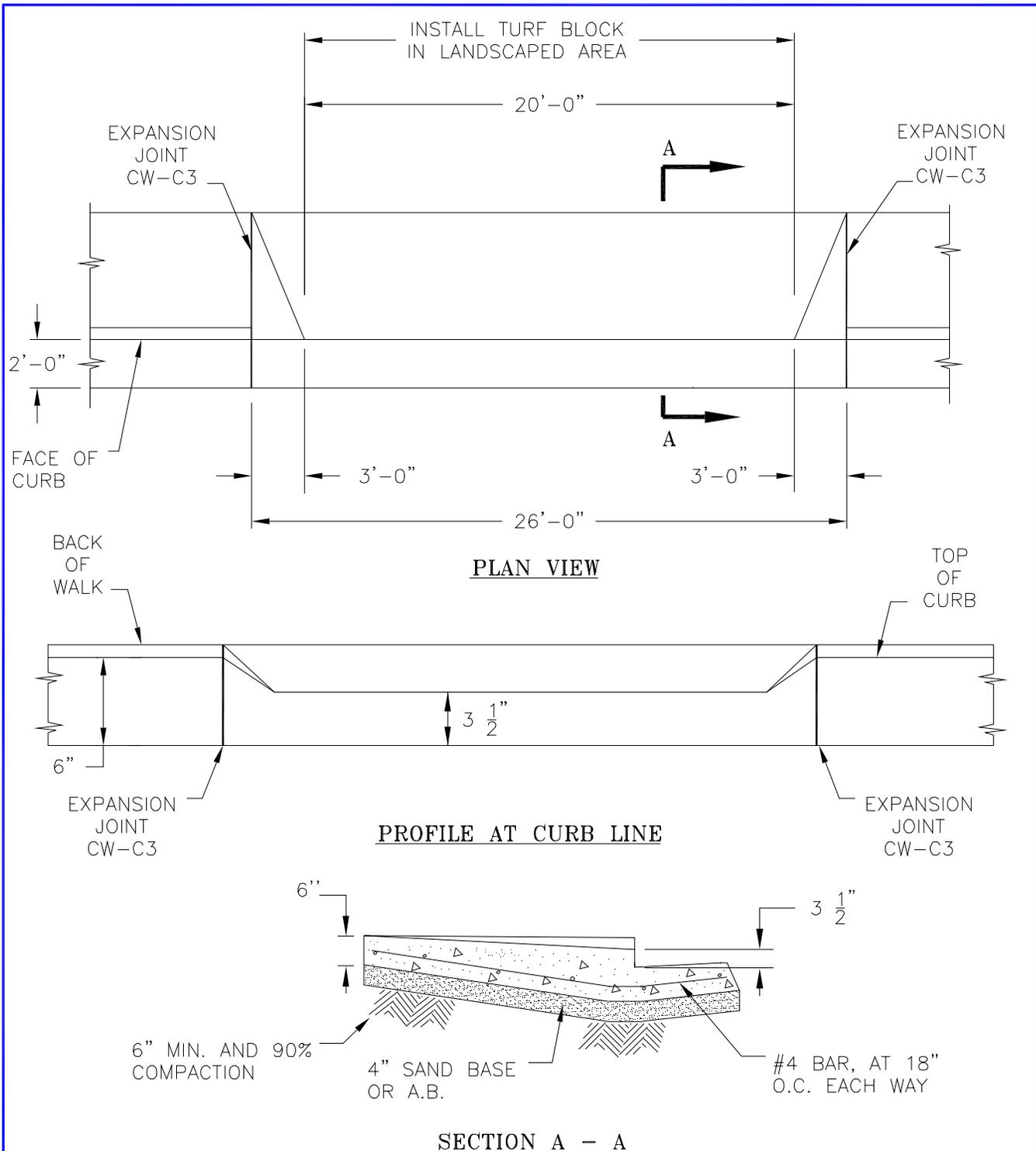


**FRONT ELEVATION**



**SECTION A - A**

REV. NO.	REV. DATE	REV. BY	<b>COMMERCIAL RAMP DRIVEWAY</b>	REVISION APPROVED BY CITY ENGINEER	
5	6/1/2000	HLE/RH		Finbar J. O'Regan DATE: 01/09/02	
DIGITIZED		7/1/91	<b>CITY OF STOCKTON</b> DEPARTMENT OF PUBLIC WORKS	SUPERCEDES DWG. DATED	DRAWING NO.
DWG. BY	RC	SCALE		2/23/95	27A
CK. BY		NONE			

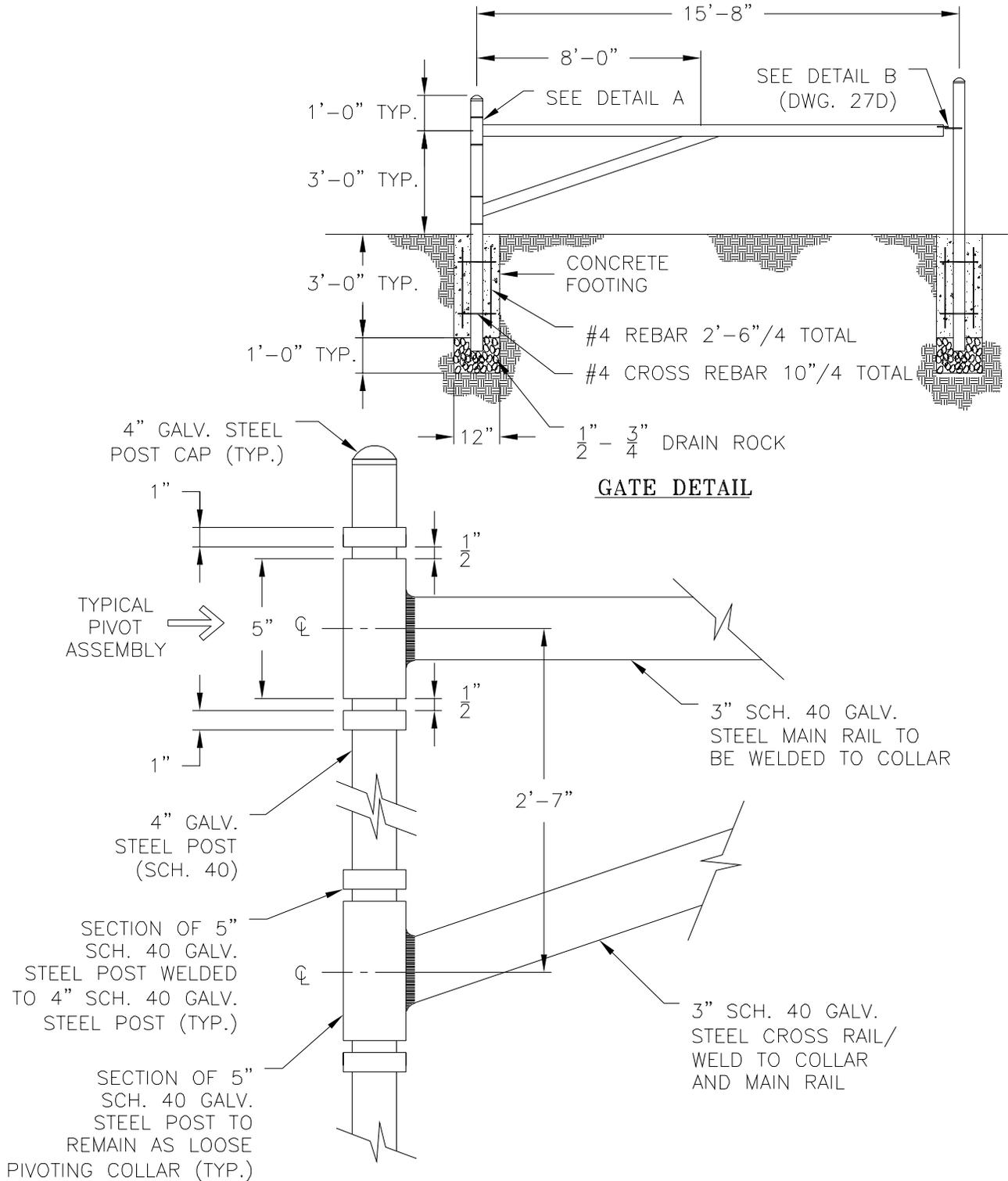


**NOTES:**

1. SEE C.O.S. STANDARD DRAWING NO. 38 FOR DETAILS NOT SHOWN.
2. INSTALL TURF BLOCK IN LANDSCAPED AREA AS REQUIRED BY CITY ENGINEER.
3. INSTALL PIPE GATE (COS STD DWG NO 27C & 27D) AS REQUIRED BY CITY ENGINEER.

REV. NO.	REV. DATE	REV. BY	<b>FIRE ACCESS DRIVEWAY</b>	REVISION APPROVED BY CITY ENGINEER	
5	6/1/2003	HL/EA		Finbar J. O'Regan	
DIGITIZED	1/1/92			DATE: 11/25/03	
DWG. BY	RC	SCALE	<b>CITY OF STOCKTON</b> DEPARTMENT OF PUBLIC WORKS	SUPERCEDES DWG. DATED	DRAWING NO.
CK. BY		NONE		01/09/02	<b>27B</b>

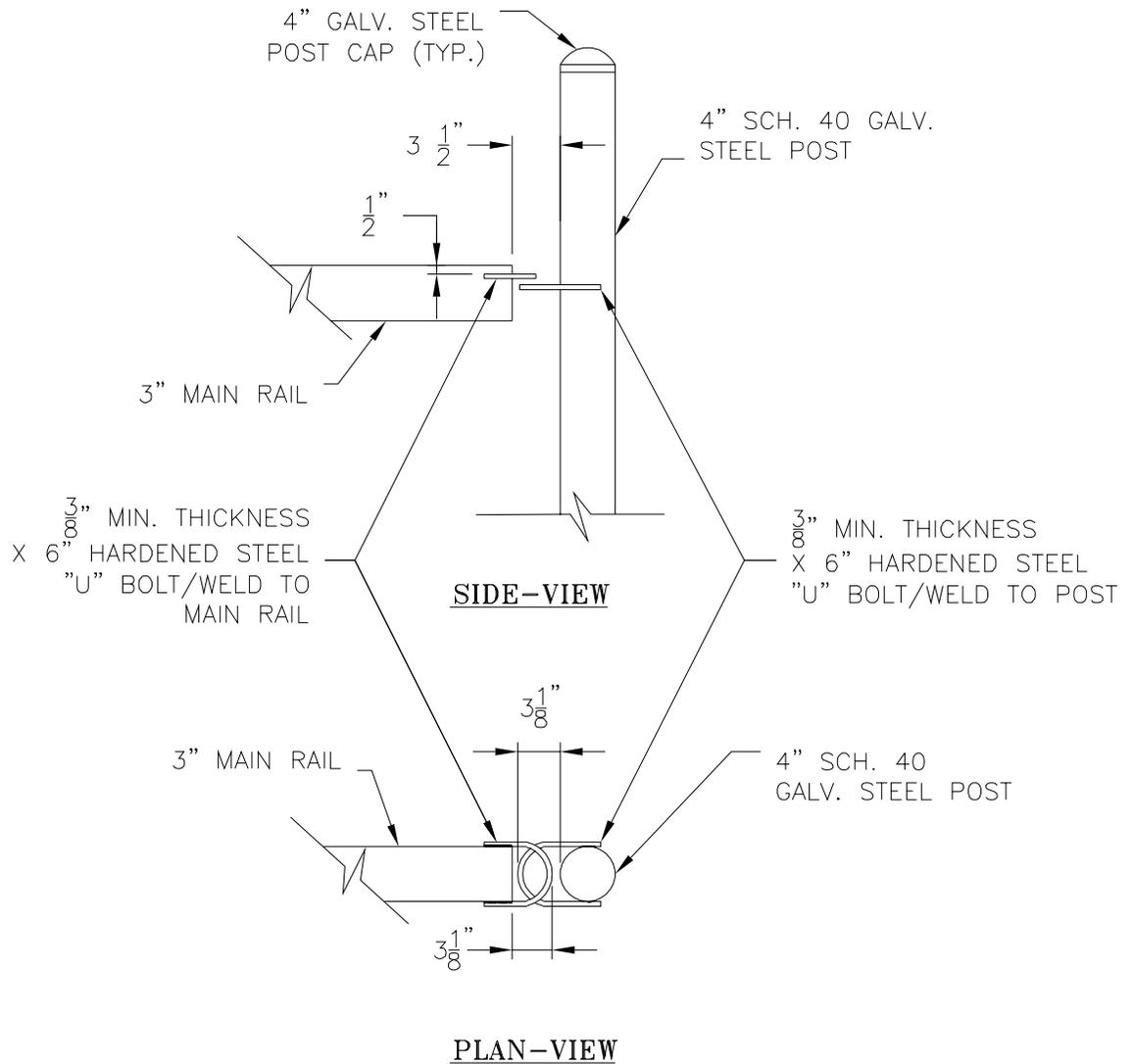
(CONT'D FROM DWG. 27B)



**DETAIL A (GATE TO POST DETAIL PIVOT)**

REV. NO.	REV. DATE	REV. BY	FIRE ACCESS PIPE GATE	NEW DRAWING APPROVED BY CITY ENGINEER	
	6/1/2000	HLE/RH		Finbar J. O'Regan DATE: 01/09/02	
DIGITIZED			CITY OF STOCKTON DEPARTMENT OF PUBLIC WORKS	SUPERCEDES DWG. DATED	DRAWING NO. <b>27C</b>
DWG. BY	HLE	SCALE			
CK. BY		NONE			

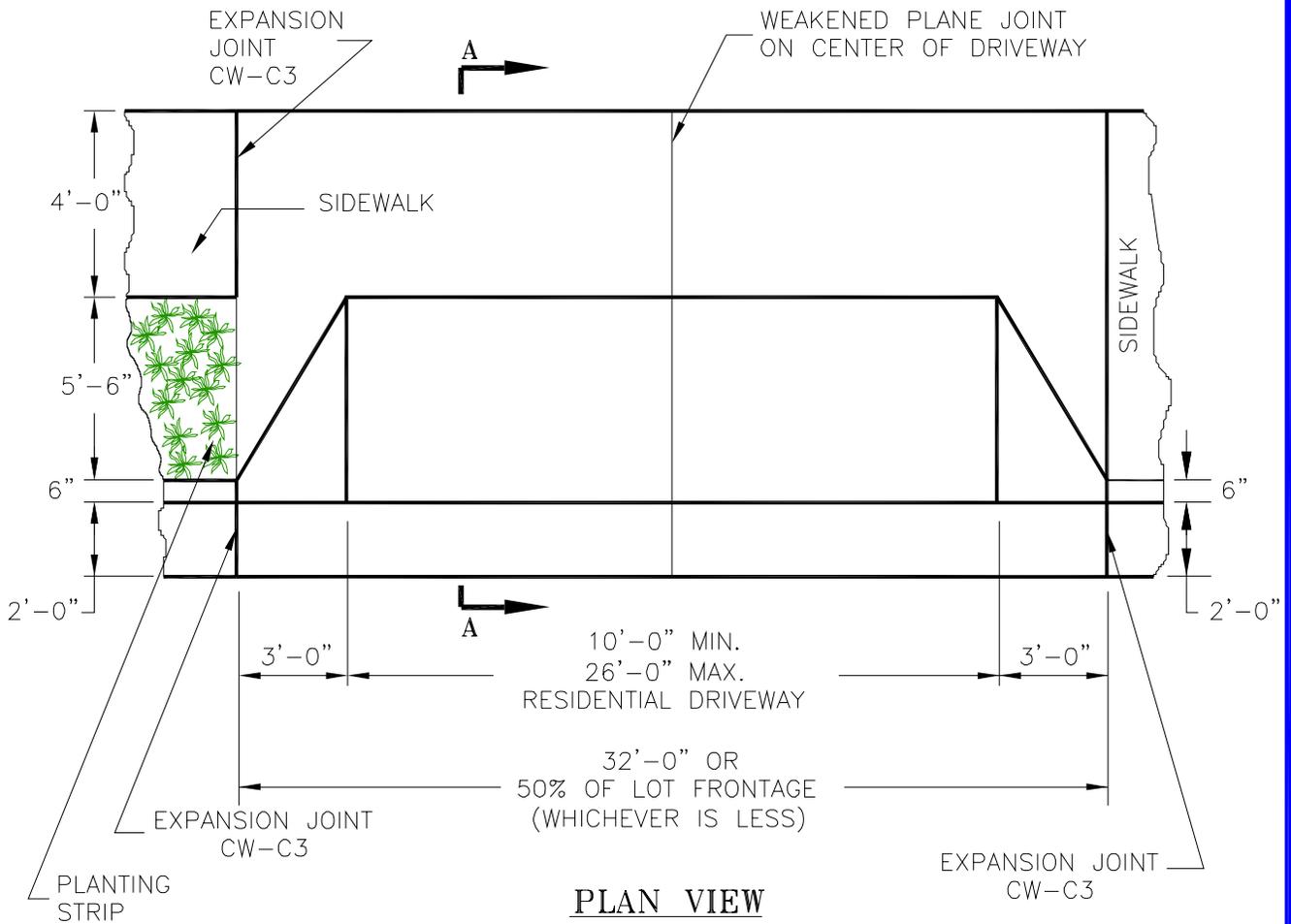
(CONT'D FROM DWG. 27C)



DETAIL B (RAIL TO POST LOCK DETAIL)

NOTE: LOCKING MECHANISM SHALL BE SUBJECT TO THE APPROVAL OF THE FIRE DEPARTMENT.

REV. NO.	REV. DATE	REV. BY	FIRE ACCESS PIPE GATE	NEW DRAWING APPROVED BY CITY ENGINEER	
	6/1/2000	HLE/RH		Finbar J. O'Regan DATE: 01/09/02	
DIGITIZED			CITY OF STOCKTON DEPARTMENT OF PUBLIC WORKS	SUPERCEDES DWG. DATED	DRAWING NO.
DWG. BY	HLE	SCALE		27D	
CK. BY		NONE			

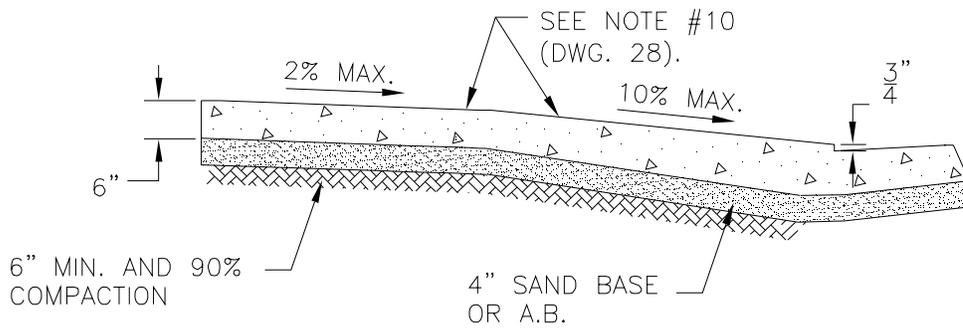
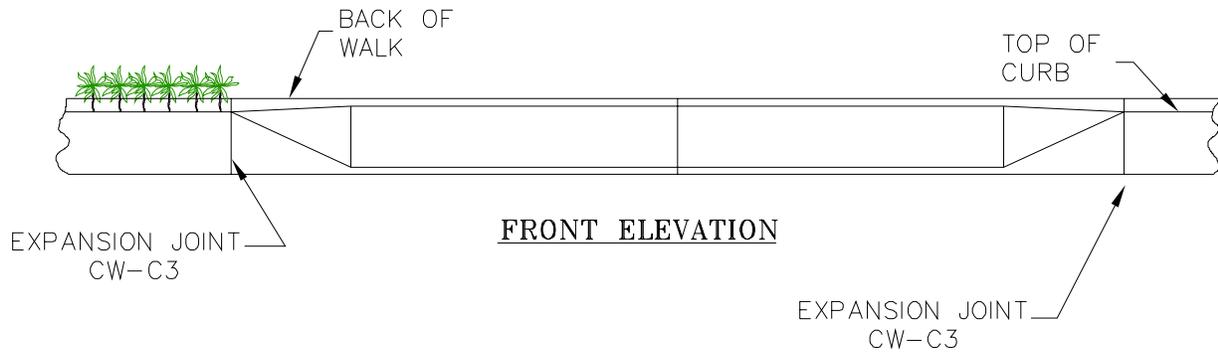


**NOTES:**

1. THIS DRIVEWAY TO BE USED ONLY WHERE APPROVED BY THE CITY ENGINEER.
2. DRIVEWAY TO CONFORM WITH EXISTING SIDEWALK. ALL CONCRETE TO BE CLASS B (5 SACK).
3. IF SIDEWALK IN BACK EXISTS, WHERE NEW RAMP TO BE PLACED. IT SHALL BE REMOVED AND REPLACED WITH 6" THICK PORTLAND CEMENT CONCRETE.
4. WHERE HIGH STREET CROWN EXISTS, DRIVEWAY RAMP MAY BE EXTENDED FROM BACK OF WALK TO LIP. DISABILITY RAMPS SHALL BE ACCOMMODATED
5. WHERE DRIVEWAY PROVIDES ACCESS TO A ONE-WAY STREET, APPROPRIATE SIGNS AS SPECIFIED BY THE CITY TRAFFIC ENGINEER WILL BE REQUIRED.
6. DRIVEWAY RAMP SHALL BE AS WIDE AS EXISTING DRIVEWAY INTO YARD, ROUNDED UP TO NEXT EVEN FOOT (10'-0" MINIMUM).
7. IN EVENT OF OBSTRUCTIONS IN PARKWAY (E.G., POWER POLES, ETC.) WIDTH MAY BE MODIFIED TO MEET EXISTING CONDITIONS AS APPROVED BY THE CITY ENGINEER.
8. PARKWAY WARP WINGS SHALL BE 3'-0" FOR 6" CURB OR HIGHER. FOR 4.5" CURB WARP WINGS SHALL BE 2'-0".
9. WHEN IT IS NOT POSSIBLE TO PROVIDE 2'-0" OF FULL HEIGHT WITH STANDARD 3'-0" WARPS ON EITHER SIDE BETWEEN ADJACENT DRIVEWAYS, A COMMON DRIVEWAY SHALL BE INSTALLED.
10. FINE HAIR BROOM FINISH.

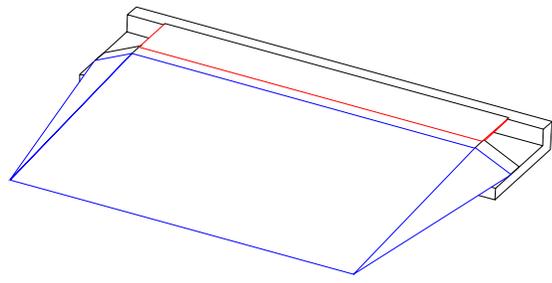
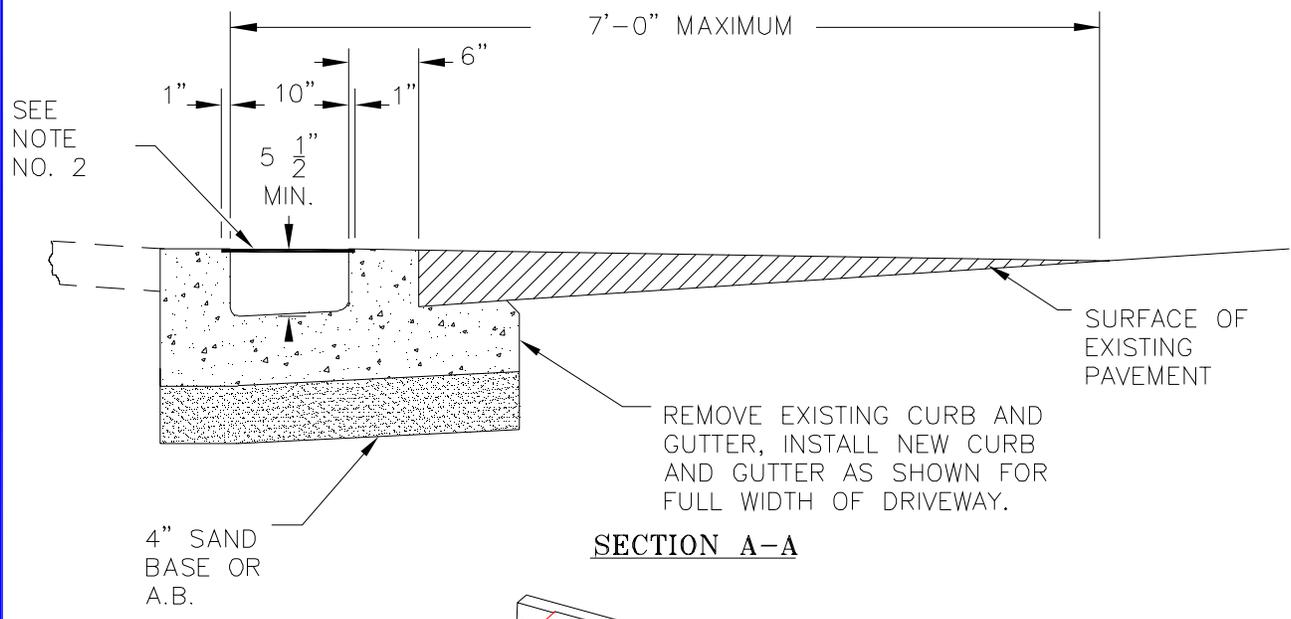
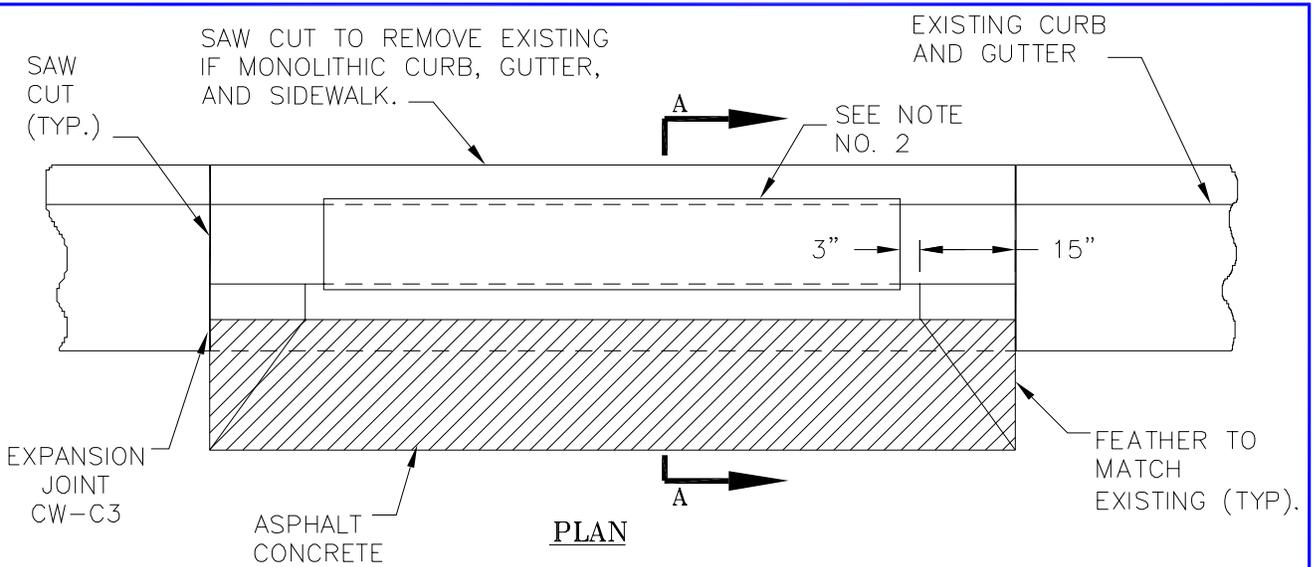
<i>REV. NO.</i>	<i>REV. DATE</i>	<i>REV. BY</i>	<b>RESIDENTIAL RAMP DRIVEWAY</b>	<i>REVISION APPROVED BY CITY ENGINEER</i>	
6	04/07/04	WW		<i>Finbar J. O'Regan</i>	
<i>DIGITIZED</i>				<i>DATE: 04/07/04</i>	
<i>DWG. BY</i>	RC	<i>SCALE</i>	<b>CITY OF STOCKTON</b> <i>DEPARTMENT OF PUBLIC WORKS</i>	<i>SUPERCEDES</i>	<i>DRAWING NO.</i>
<i>CK. BY</i>		NONE		11/25/03	28

(CONT'D FROM DWG. 28)



**SECTION A - A**

REV. NO.	REV. DATE	REV. BY	RESIDENTIAL RAMP DRIVEWAY	REVISION APPROVED BY CITY ENGINEER		
6	04/07/04	WW		Finbar J. O'Regan		
DIGITIZED	7/1/91		CITY OF STOCKTON DEPARTMENT OF PUBLIC WORKS	DATE:	04/07/04	
DWG. BY	RC	SCALE		SUPERCEDES DWG. DATED	DRAWING NO.	
CK. BY		NONE		01/09/02	28A	



**NOTES:**

**3D-PERSONAL VIEW**

1. PROPERTY OWNER SHALL BE RESPONSIBLE FOR KEEPING GUTTER CLEAN UNDER STEEL PLATES.
2. 1/2" x 12" STEEL PLATES FOR FULL WIDTH OF DRIVEWAY. MAXIMUM LENGTH OF EACH PLATE 6'-0".
3. FINE HAIR BROOM FINISH ON CONCRETE SURFACES.
4. THIS DRIVEWAY TO BE USED ONLY WHERE APPROVED BY THE CITY ENGINEER.

REV. NO.	REV. DATE	REV. BY	<b>BRIDGE-TYPE-DRIVEWAY</b>	REVISION APPROVED BY CITY ENGINEER	
4	6/1/2000	HLE/RH		Finbar J. O'Regan DATE: 01/09/02	
DIGITIZED		7/1/91	<b>CITY OF STOCKTON</b> DEPARTMENT OF PUBLIC WORKS	SUPERCEDES DWG. DATED	DRAWING NO.
DWG. BY	RC	SCALE		10/7/93	29
CK. BY		NONE			